

Title: Traffic Management for Single Vehicle Works

PCoP: G:9

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PLACE HEALTH AND SAFETY MANUAL

1. Introduction

In accordance with the Health and Safety at Work Act 1974 (HASAWA) and Lincolnshire County Council's Environment and Economy Directorate requirements this Code of Practice shall be observed when undertaking works on behalf of Lincolnshire County Council (LCC) Highways that involve traffic management for single vehicles.

Under HASAWA an employer has a duty to provide instruction and training in health and safety to its employees. This Code of Practice (CoP) provides a means towards meeting this responsibility.

Safe systems of work shall be put in place and maintained to ensure the safety of the public, road user, persons undertaking site work and traffic management providers.

Safety at Street Works and Road Works, A Code of Practice (The Red Book) now prescribes definitions of mobile works and short duration works. Often the works we routinely undertake either encompass both of these types of traffic control or fall between the two.

This CoP is intended to provide a safe system of work for operations where the activity is that which would be defined as short duration but there are multiple sites within a particular stretch of carriageway, e.g. gully and offset cleansing, multiple pot holes, road stud works, white lining, provision and maintenance of grips, etc.

Some short duration works may be undertaken without the use of static signs or cones provided that a site specific risk assessment demonstrates that traffic can pass the works vehicle safely and without difficulty, and there is a low risk to operatives and road users e.g. Gully emptying along a single carriageway road where operatives are working wholly on the footway or verge (refer to Red Book p.91). Further guidance is provided in Chapter 8 O8 Single Vehicle Works.

This code does not cover continuous mobile works or single sites of short duration. The requirements of the red book adequately cover this.

The Red Book sets out the principles to follow when signing, lighting and guarding works on the highway. It states that it provides guidance on the requirements but cannot cover every situation and a site-specific risk assessment must be undertaken for all work sites.

This CoP is intended to provide supplemental guidance for works so that they meet the requirements in the red book. However, you are still expected to undertake a dynamic risk assessment to ensure that the layout used is suitable for the site. As a minimum, significant changes need to be recorded. Your organisation or business stream may also require that a risk assessment is recorded for all sites.

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Reference should be made to CPE9 – 'The Management of Contractors and Subcontractors regarding competency to carry out such works' ([CPE9](#)).

2. Definitions

For the purposes of this CoP the definitions of mobile works and short duration works are as defined in the red book.

Mobile works are carried out from a vehicle moving significantly more slowly than the prevailing traffic speed and involve continuous mobile operations; they include grass cutting, hedge cutting and weed spraying.

Short duration works involve a single vehicle or a small number of vehicles undertaking one or more intermittent stops of:

Up to 15 minutes – used for activities such as street lighting maintenance, pole testing, road marking, compliance testing, jetting of sewers, jet or velocity patching, etc.; or

Between 15 and 60 minutes – could be used for activities such as pot hole repairs, leak detection, fault finding, tree cutting or felling, installation of traffic loops, coring, etc.

This time period must include all time needed to set up and dismantle signing, lighting and guarding.

Single vehicle works are those works which involve a vehicle either standing for a short duration or operating at a low speed in a carriageway (Chapter 8 08.1)

3. Scope and Application

This CoP is intended for use within the planning process and during the site works for single vehicle works on a single carriageway road that include multiple, intermittent, short duration works. The CoP shall be used in conjunction with the Red Book and Chapter 8. If single vehicle works are to be carried out on dual carriageways or carriageways with three or more traffic lanes the traffic management must be considered in accordance with Chapter 8 incorporating lane closures and positive traffic control.

Lincs Laboratory have their own CoP, Traffic Management for Laboratory Operations ([CPF3](#)), that exceeds the minimum standards within this code.

4. Essential Requirements

- i) Each site shall be assessed before work is programmed to determine which traffic management option shall be used. These assessments will consider the site circumstances e.g. location of the work, the traffic speed and volume, the time of day the works are due to take place, the

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generic risk assessment if applicable to the activity to be undertaken on site and this CoP. Where required a job specific risk assessment shall be completed.

- ii) Accident Protection Vehicles (APV) (Crash Cushion) are normally only used on dual carriageways. The use of APV or other suitable cover vehicles shall be determined by risk assessment on single carriageways. APV are not normally used for works on single carriageways, however where necessary a case for using them will need to be made within the risk assessment. At sites where an APV are being considered due to the risk to operatives or members of public a minimum of layout 2 (Page 6) must be implemented.
- iii) Whenever possible works shall be programmed with other works, e.g. planned cyclic works where designed temporary traffic management arrangements are already in place.
- iv) All sites shall have appropriate traffic management in place for the works taking place to reduce the risk so far as is reasonably practicable.
- v) Wherever possible work shall be undertaken when there is good visibility along the site. Where visibility is reduced by bends or hills additional warning signs shall be used.
- vi) Approved performance reflective clothing and equipment shall be worn at all times, see Directorate Code of Practice [CPA5](#).
- vii) Where adverse weather conditions are encountered all work shall be suspended.
- viii) Particular care shall be taken at junctions to ensure that drivers entering onto the area covered by traffic management are made aware of the works. This may require the use of additional warning signs.
- ix) If working on or near to footways, always confirm there is no danger for pedestrians. Pedestrians should always have a clear route and must never be diverted onto an unguarded carriageway.
- x) For works on or near to a level crossing a site specific risk assessment shall be completed prior to commencement of the works. See Directorate Code of Practice CPA8 for further detail. Some level crossing sites will require the attendance of the rail operator this will be determined at design stage. All personnel attending site shall be briefed on the contents of the risk assessment for that site.
- xi) Any queries or doubts about the application of this CoP should be referred to a Line Manager/Supervisor or your Safety Advisor.

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5. Traffic Management Layout

The layouts below should be suitable for most operations within the scope of this CoP. However, any additional signage etc. will be determined by site-specific risk assessment.

Layout 1 is the basic layout and may be used if, following a risk assessment, it is not necessary to use traffic control by means of manually operated "stop/go" boards.

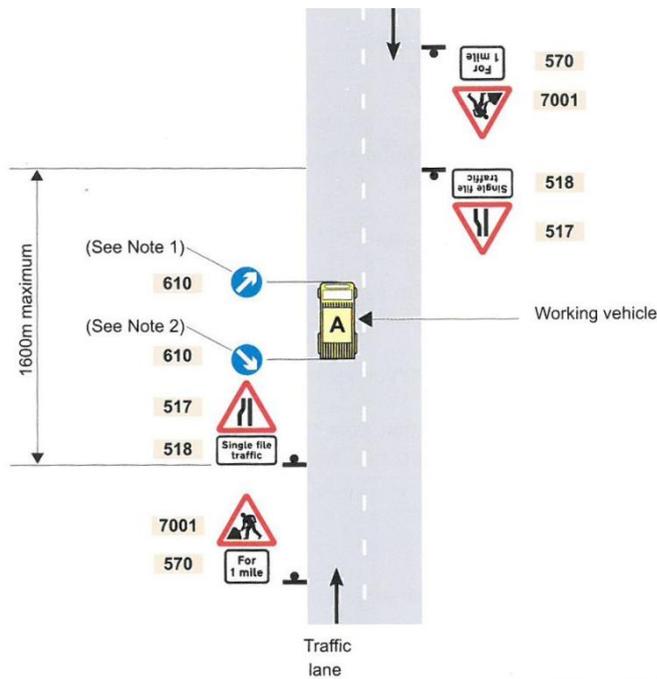
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Layout 2 shows an example of a traffic management plan for single vehicle works on single carriageways using traffic control. Such arrangements should be used when forward visibility is limited to less than 150m or site circumstances require traffic control.

Insert amended SWV1 and SVW2 from Chapter 8 (p 89 & p 91)

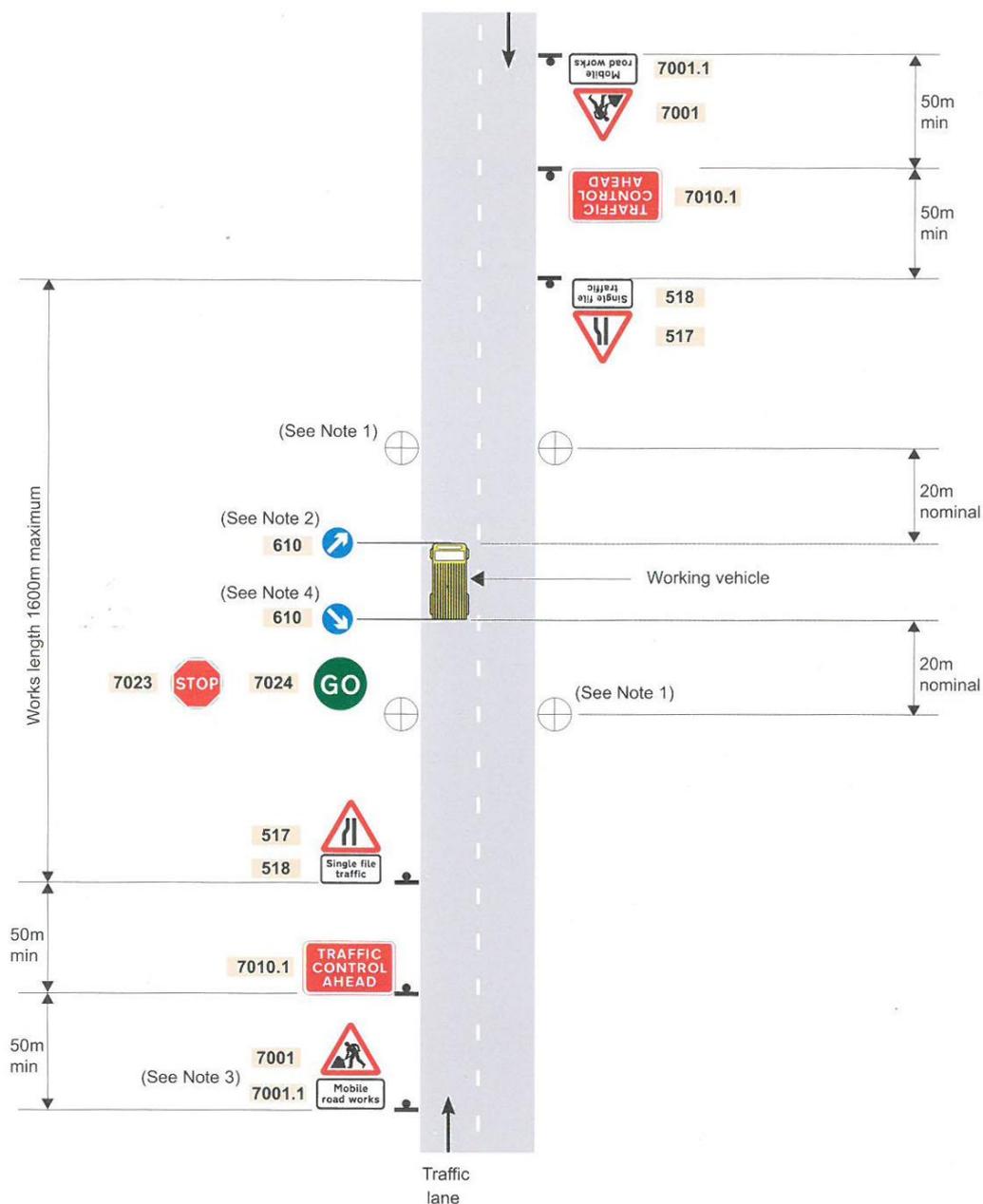
Layout 1

Plan SVW1: Single vehicle works on single carriageway roads, basic layout



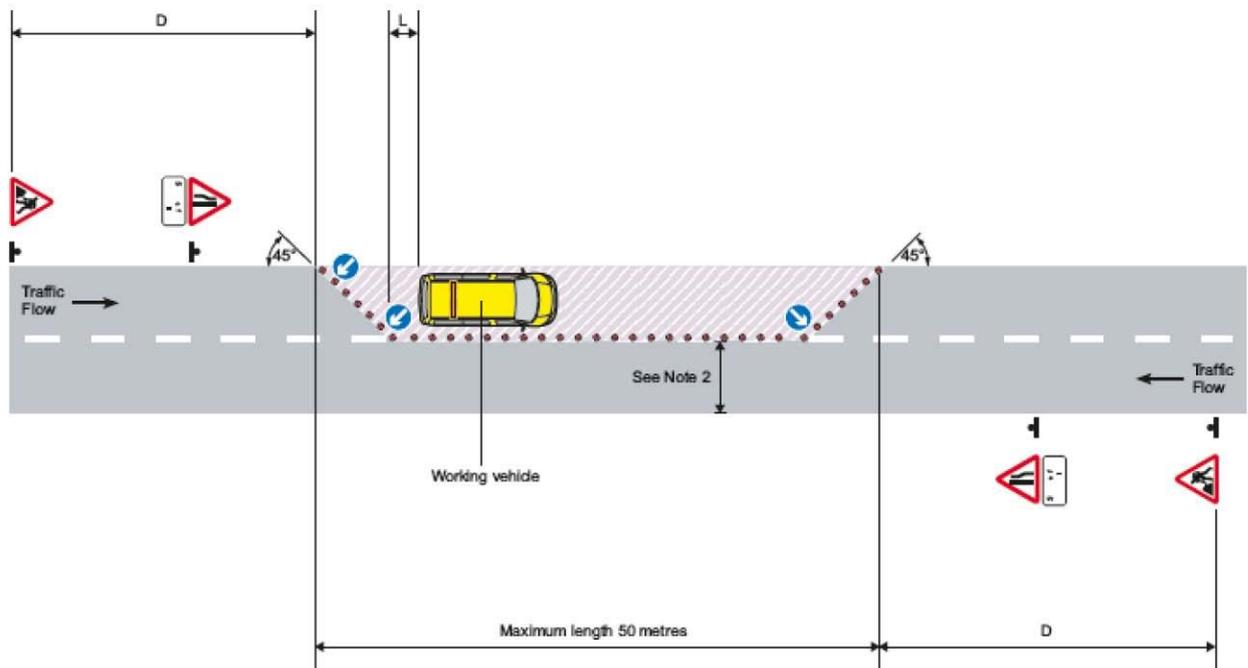
Layout 2

Plan SVW2: Single vehicle works on single carriageway road – "STOP/GO"



Works length = 1 mile maximum.

Short duration stops more than 15 minutes on a single carriageway road



Notes

- 1 For dimensions D and L see table inside back cover.
- 2 See page 52 for guidance on unobstructed width past the works.

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